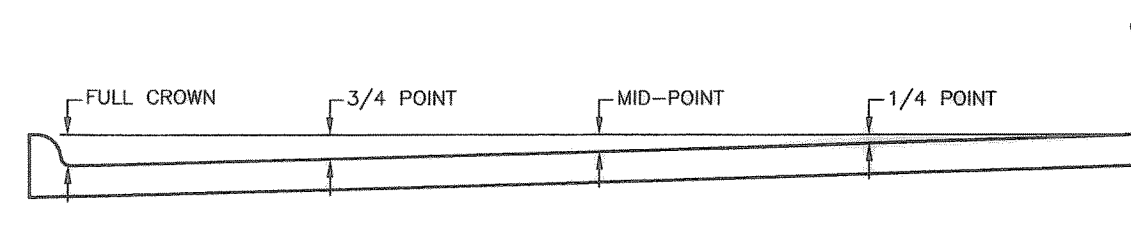


STREET TYPE	STREET WIDTH (W)	A	B	R.O.W. WIDTH	P
COLLECTOR	36'	8'	10'	60'	12'
COLLECTOR	44'	10'	12'	65'	10'-6"
COLLECTOR	48'	12'	12'	69'	10'-6"

FOUR MOVING LANES OR
TWO MOVING LANES / TWO PARKING LANES

- ① SAWED LONGITUDINAL DUMMY JOINT.
- ② CONSTRUCTION JOINT (FULL WIDTH PAVEMENT IS ALLOWED WHERE APPROVED BY ENGINEERING DEPARTMENT).
- ③ ALL REINFORCING BARS SHALL BE NO. 3 TRAVERSE BARS TO BE SPACED ON 1'-6" CENTERS, LONGITUDINAL BARS TO BE SPACED ON 1'-6" EXCEPT WHERE NOTED.
- ④ A NO. 3 BAR, CONTINUOUS, SHALL BE PROVIDED FOR CURB REINFORCEMENT.

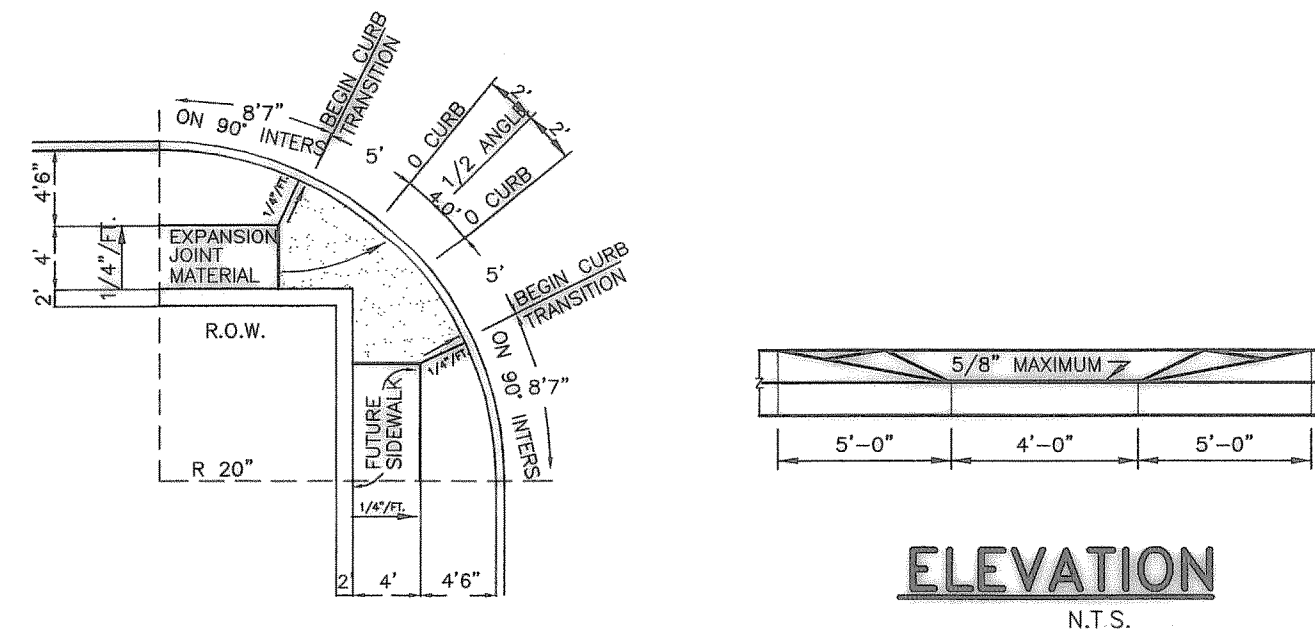


SLIP-FORM PAVEMENT MUST MEET CROWN GRADES AT GUTTERS, AT MID-POINTS AND L.

WIDTHS OF PAVEMENT ARE FACE TO FACE

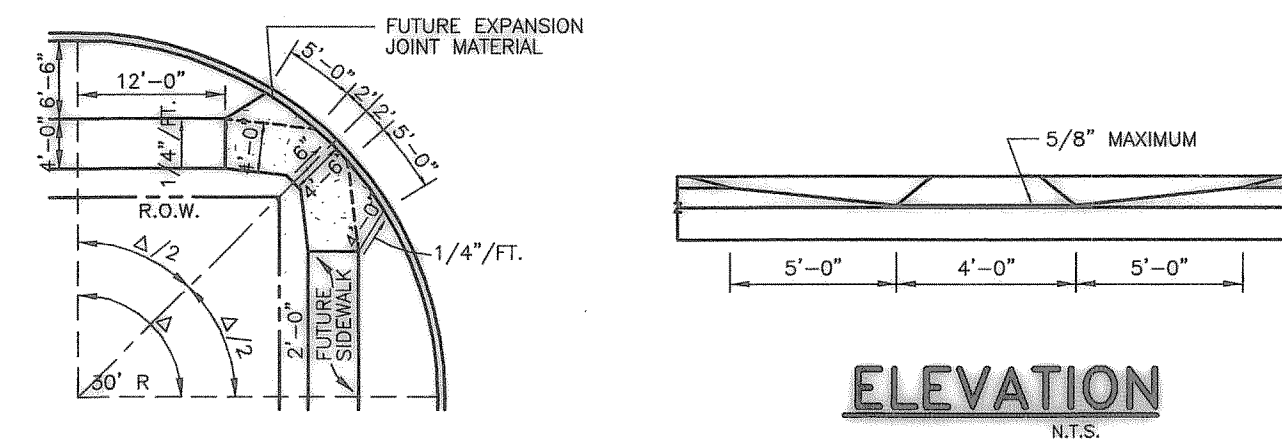
ROADWAY WIDTH (W)	TOTAL CROWN HEIGHT	3/4 POINT	MID-POINT	1/4 POINT
26'	4"	3"	2"	1"
36'	7"	3 - 7/8"	1 - 3/4"	3/8"
44'	7"	3 - 7/8"	1 - 3/4"	1/2"
48'	7"	3 - 7/8"	1 - 3/4"	1/2"
60'	7"	3 - 7/8"	1 - 3/4"	1/2"
2-34'	5"	3 - 3/8"	1 - 1/2"	3/8"

TABLE OF CROWN HEIGHTS AND ORDINATES FOR VARIOUS PARABOLIC SECTIONS



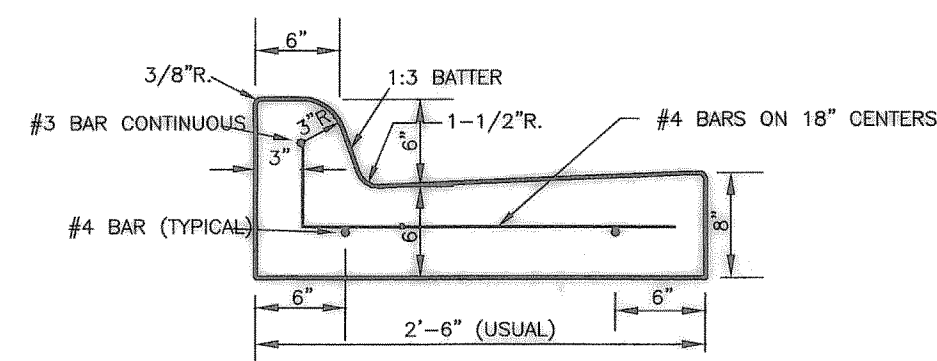
PLAN VIEW

BARRIER FREE RAMP AT MINOR INTERSECTION

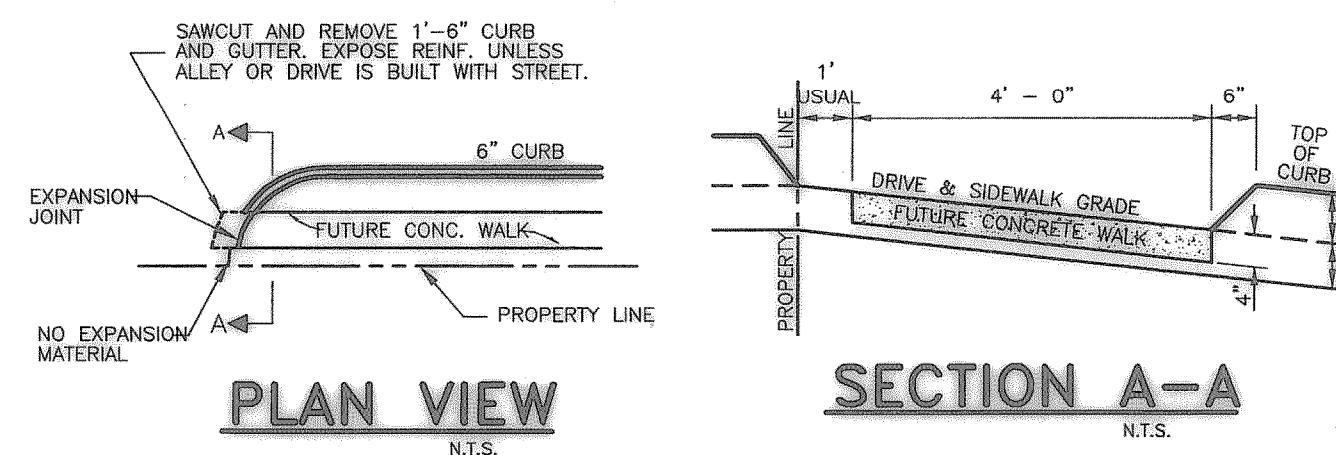


PLAN VIEW

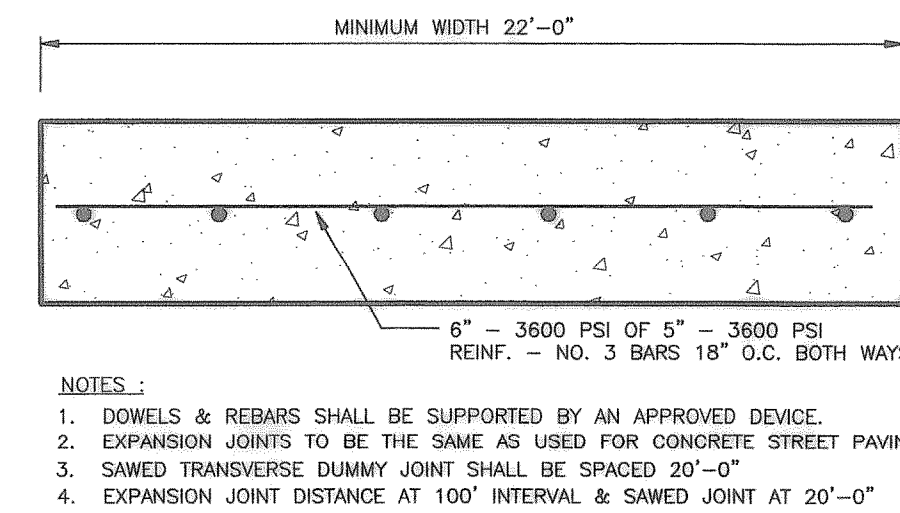
BARRIER FREE RAMP AT MAJOR INTERSECTION



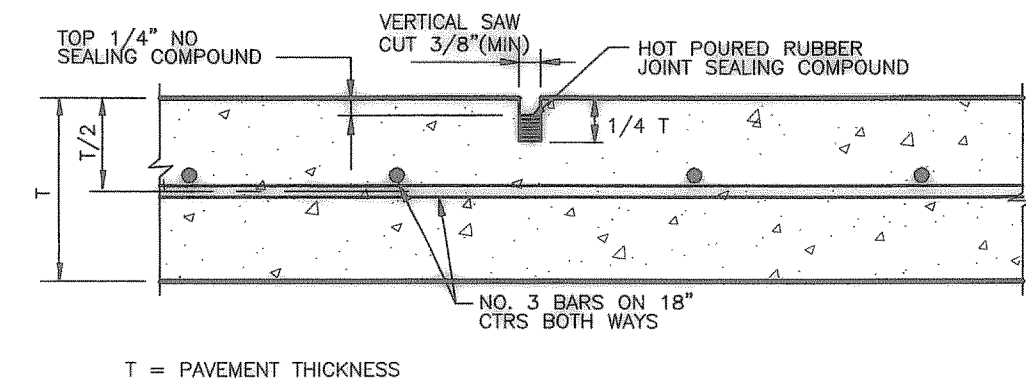
SEPARATE CURB-AND-GUTTER



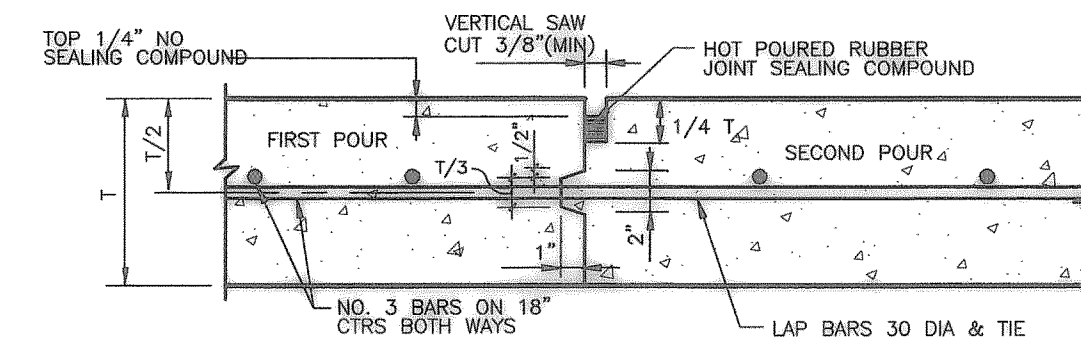
BARRIER FREE RAMP AT DRIVE OR ALLEY



FIRE LANE PAVING & JOINT DETAIL



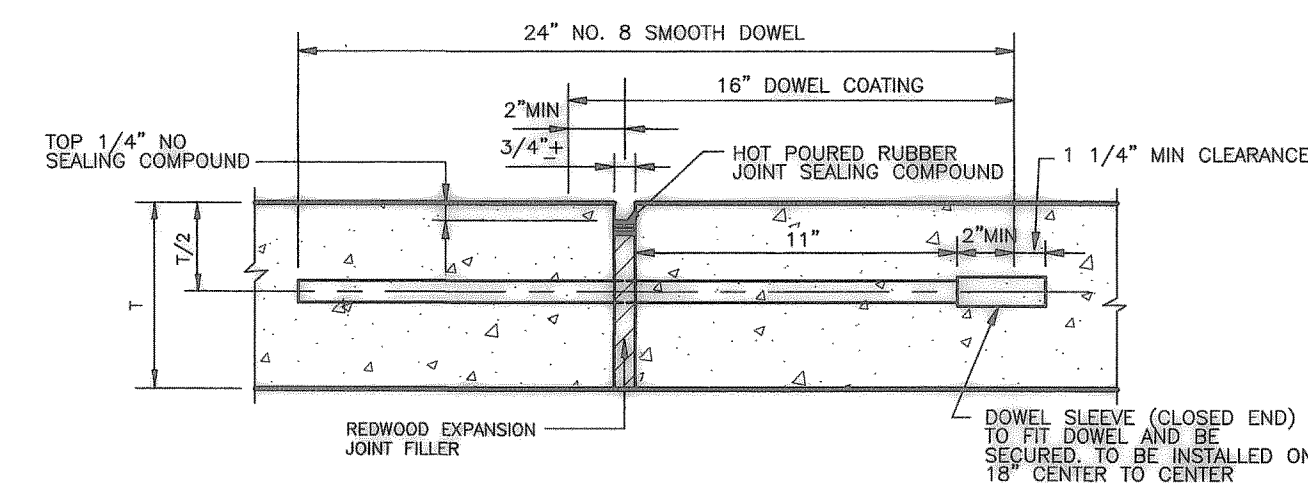
SAWED DUMMY JOINT



CONSTRUCTION JOINT FOR PAVEMENT

TRANSVERSE AND LONGITUDINAL JOINTS

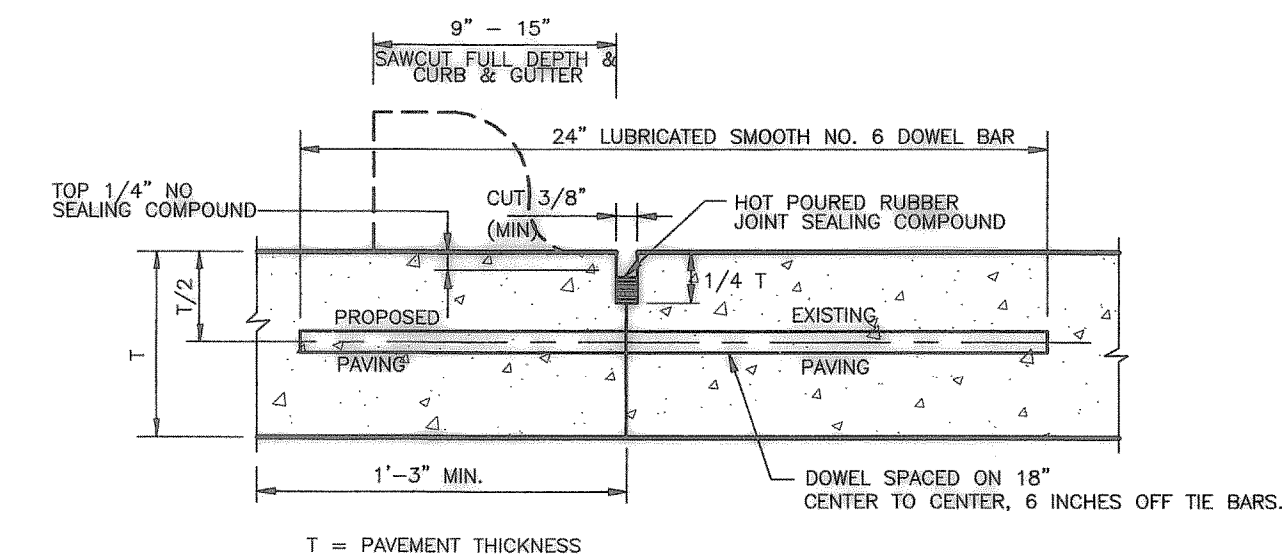
NOTE :
CONTRACTOR SHALL PROTECT KEYWAY PRIOR TO SECOND POUR. IF LONGITUDINAL KEYWAY IS DAMAGED, CONTRACTOR SHALL REPAIR WITH THE USE OF LONGITUDINAL BUTTE JOINT. DRILL AND GROUT DOWELS INTO FIRST POUR.
T = PAVEMENT THICKNESS



NOTE:
DOWELS AND REINFORCING BARS SHALL BE
SUPPORTED BY AN APPROVED DEVICE.

TRANSVERSE EXPANSION JOINT

(SPACED 600 FT. MAXIMUM, LOCATE AT INTERSECTIONS)
T = PAVEMENT THICKNESS



PAVEMENT THICKNESS

ROADWAY	8"	-	3,600 PSI	MIN. 6 SACK MACHINE POUR	6.5 SACK HAND POUR
DRIVEWAY	6"	-	3,600 PSI	MIN. 6 SACK MACHINE POUR	6.5 SACK HAND POUR

LONGITUDINAL BUTT JOINT

NOTES :

1. NO. 5 SMOOTH DOWEL BAR MAY BE USED IN 5 INCH AND 6 INCH PAVEMENT THICKNESS.
2. LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTORS OPTION.
3. DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIG.
4. DRILLING BY HAND IS NOT ACCEPTABLE, PUSHING DOWEL BARS INTO GREEN CONCRETE NOT ACCEPTABLE.
5. NO SAND UNDER PAVEMENT

RECORD DRAWINGS
AS PROVIDED BY:
I.C.S. COMMERCIAL

Michael S. Allen
MICHAEL S. ALLEN, P.E.
DATE: 9/25/03